



Construction Traffic Management Plan for 32 Bury Walk Project

CONSTRUCTION TRAFFIC MANAGEMENT PLAN (CTMP)

- Q1.
- Please confirm that you have read and understood the Council’s Code of Construction Practice and the Council’s planning guidance on construction traffic management set out within the Transport and Streets SPD and the Basements SPD.
 - Please confirm that in preparing your CTMP proposal you have reviewed any Construction Traffic Management Plans (CTMP) that have been approved at nearby properties. These can be found using an online map based tool available [here](#).
 - Please confirm that you have read the guidance notes on how to complete this proforma included on the final page of this document.
 - Please note the approval of a CTMP does not remove the need to obtain highways licences and any other approvals that might be required.

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PLAN IDENTIFICATION

- Q2. Provide a date of issue for this document and, if relevant, a revision number.

ISSUE NO: Version 1 by Seneca Services DATE: 19th March 2025. ISSUE NO: Version 2 by Liongate Construction (This Report) DATE: 18 th August 2025 Main changes in this version are on spoil removal plan/vehicle and related sections.

- Q3. What is the full postal address of the site?

ADRESS: 32 Bury Walk, London SW3 6QB

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Figure 1: Site Location



Q4. Please provide the planning permission reference number for the development. Otherwise please confirm this is a Draft CTMP to accompany a planning application.

PLANNING REFERENCE NO: PP/23/01046

Q5. Please give a very brief description of the work.

Construction of a two-storey one-bed house, with a half basement on this vacant site; replacement of existing tree.

Q6. Please provide contact details for the person responsible for completing this form.

NAME: Charlie Abdullah
COMPANY: Liongate Construction
POSITION: Construction Management Consultant
TEL: 07496 788940
EMAIL: charlie@liongateconstruction.co.uk

CONSTRUCTION TRAFFIC MANAGEMENT PLAN (CTMP)

PRE SUBMISSION NEIGHBOUR CONSULTATION

Please detail how neighbours have been involved in the development of this plan. Please confirm you have contacted the Residents' Association for the street (if there is one). Please identify whom you consulted on what dates and by what means. Please append any responses received.

Local people understand the local context and can provide constructive and valuable advice on how best to carry out a development given the context. We recommend that neighbours be contacted in writing with outline proposals and that they be given a minimum of two weeks to respond with their views in advance of any formal submission to the Council. Any consultation responses submitted to the Council in respect of a Draft CTMP should be used to inform the proposals and must be responded to in the Final CTMP. Details of the Borough's Residents' Associations can be found [here](#).

7.1 Pre-Commencement Neighbourhood Consultation

a. A copy of the Draft CTMP Consultation Letter was emailed to the following on **28th February 2025**.

- Local residents association (mail to Mrs Susan Spiller chair@ssdra.org.uk)
- Marlborough Primary School (mail to info@marlborough.rbkc.sch.uk)
- Oratory Primary School (mail to Ana Pavia ana.paiva@oratory.rbkc.sch.uk)

b. A copy of the Draft CTMP Consultation Letter was hand delivered on the 28th February 2025 to the addresses detailed below:

No's 24, 25, 26, 28,30, 33, 34, 35, 37, 39, 41, 43, 44 Bury Walk. Furthermore, the entire CTMP was forwarded to the PA of the owner of 30 Bury Walk. The Draft CTMP was also explained in person to Mr Hakim Hassani who often presents himself as the representative of the residents in Bury Walk and the resident of No. 33 Bury Walk.

c. The 14-day neighbourhood consultation period ran from 28th February 2025 to 14th March 2025.

d. **No comments or request for information** were received during the consultation period.

A separate consultation will be carried out for this version of the CEMP, and any comments will be added here.

CONSTRUCTION TRAFFIC MANAGEMENT PLAN (CTMP)

ROUTEING OF DEMOLITION, EXCAVATION AND CONSTRUCTION VEHICLES

Q8. Please describe the construction traffic route to be used to and from the site, showing details of links to the strategic road network (A and B roads) and highlighting any nearby cycling facilities (including designated quiet ways and roads with contraflow cycling) that would be affected. **Provide a plan** (numbered and dated with a revision number if necessary) illustrating these details. Construction traffic on other routes is not permitted. The route described must be adhered to.

The route should avoid residential side streets wherever possible and vehicles should, in most circumstances, approach the site from the left hand side of the road in two-way streets. A plan of the route must be forwarded to visitors and delivery companies in advance. The route should be able to accommodate all vehicles visiting the site in terms of capacity, geometry, width and height. If necessary, submit swept paths to demonstrate the suitability of the proposed route. Consider any sensitive sites or major trip generators (e.g. schools, offices, public buildings, museums, etc.) on the route or nearby, and other planned developments and developments under construction - can they be avoided? Vehicles must not drive on footways other than at dedicated access points.

8.1 Construction Vehicle Access and Egress – Route 1 (Bury Walk)

The following site access and egress route will be used for spoil removal and plant and material delivery vehicles only.

Site Access (Yellow Arrow):

1. Approach site from Fulham Road (A308) and turn into Pond Place.
2. Continue in a southerly direction on Pond Place before turning right into Bury Walk and continuing right into Bury Walk before pulling up directly outside 32 Bury Walk (narrow bodied vehicles).

Site Egress (Blue Arrow):

3. Exit site and continue in a northerly direction towards the junction with Fulham Road (A308).
4. Enter Fulham Road (A308) and continue away from site.

NB: Links to the strategic road network, Fulham Road (A308), is shown in red within figure 2.

CONSTRUCTION TRAFFIC MANAGEMENT PLAN (CTMP)

Figure 2: Construction vehicle access and egress – Route 1 (Bury Walk)



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8.2 Construction Vehicle Access and Egress – Route 2 (Sydney Street)

The following site access and egress route will be used for concrete delivery vehicles only.

Site Access (Yellow Arrow):

1. Approach site from Fulham Road (A308) turn into Sydney Street and continue in a southerly direction before pulling up within the designated concrete vehicle loading/unloading area (x4 suspended parking bays) directly outside 32 Sydney Street.

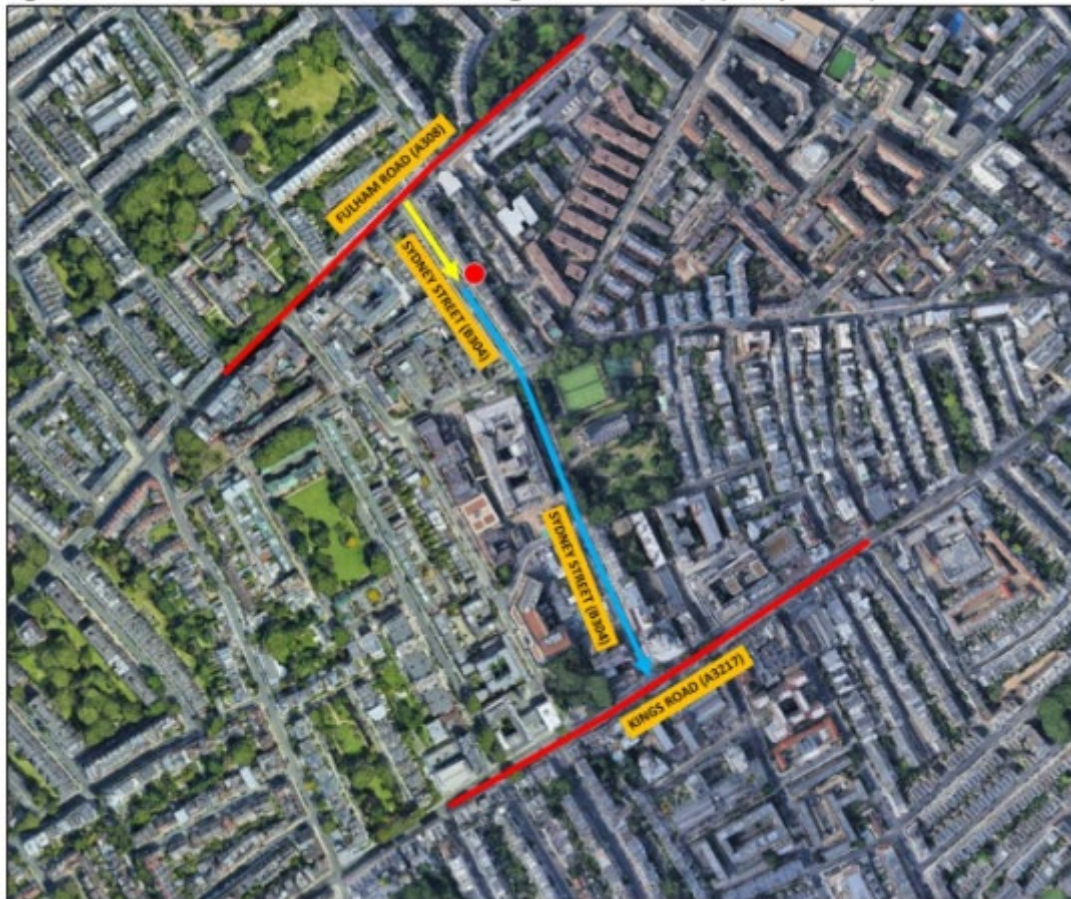
Site Egress (Blue Arrow):

2. Exit the designated concrete vehicle loading/unloading area (x4 suspended parking bays) and continue in a southerly direction towards the junction with Kings Road (A3217).

3. Enter Kings Road (A3217) and continue away from site.

NB: Links to the strategic road network, Fulham Road (A308) and Kings Road (A3217), are shown in red within figure 3.

Figure 3: Construction vehicle access and egress – Route 2 (Sydney Street)



Please refer to:

Appendix A: Swept Path: Access & Egress Route 1

Appendix B: Swept Path: Access & Egress Route 2

Appendix C: Site Layout Plan

Appendix D: Spoil Removal Plan

Appendix E: Plant & Material Delivery Plan

Appendix F: Concrete Delivery Plan

CONSTRUCTION TRAFFIC MANAGEMENT PLAN (CTMP)

Q9. Please confirm that all contractors, sub-contractors, delivery companies and visitors will be advised of and required to strictly adhere to the specified route and all the other terms of this plan.

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CONSTRUCTION TRAFFIC HOURS

Deliveries and collections must be restricted to between 9.30am and 4.30pm, Monday to Friday. Where there is a school on route, then deliveries must be restricted to between 9.30am and 3pm, Monday to Friday, during term time. Deliveries and collections are not permitted on Saturdays, Sundays or public holidays. In some particularly sensitive locations, for example where there is a street market nearby, further restrictions will be necessary. The Council will not agree to construction traffic using Portobello Road or Golborne Road on Fridays.

Please identify schools, nurseries, markets, tourist attractions or other sensitive uses near the construction site which have informed the construction traffic hours, specified below.

Oratory Roman Catholic Primary School is located at the southern end of Bury Walk. Royal Brompton National Heart and Lung Hospital is located at the Sydney Street and Cale Street junction.

Acknowledging the restrictions on hours and days given above and considering nearby uses, please specify the hours during which construction traffic to and from the site is planned. Please enter a start time and an end time in the box below.

PERMITTED HOURS FOR DELIVERIES AND COLLETCTIONS	
Monday – Friday (During Term Time)	09:30 – 15:00
Monday – Friday (Outside Term Time)	09:30 - 16:30
Saturdays	Not Permitted
Sundays	Not Permitted

Any exceptions to the above must be specified here (for example where the delivery of abnormal loads such as tower cranes, heavy plant or site offices is planned).

N/A.

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SITE ACCESS

Q11 Please supply an accurate (to scale) numbered and dated site plan annotated with dimensions showing;

- all points of site access (vehicular and pedestrian);
- where materials, skips and plant will be stored;
- position of hoarding;
- position of nearby trees (and tree protection structures);
- where construction vehicles would wait to load/unload;
- surrounding properties and their accesses;
- the layout of any approved construction sites on adjacent frontages;
- parking bay suspensions;
- a minimum of 1.2m clear footway width to be retained at all times and;
- a minimum of 3m clear carriageway width to ensure that development activity does not block the road.

Please provide the relevant drawing number (s).

The placing of welfare facilities, skips, plant and material should be on the site itself wherever possible. Their placement on the highway in front of adjoining properties will be unacceptable. The placing of welfare facilities, skips, plant or material on the highway in a position that would hinder access to surrounding properties will be unacceptable.

A minimum of 1.2m clear footway width is required to allow wheelchair users and push chairs to pass. A minimum of 3m clear roadway width must be maintained to prevent the road becoming blocked. We will require the use of narrow body construction vehicles where 3m clear width cannot be achieved with larger construction vehicles. Narrow bodied construction vehicles are those that can be accommodated fully within on street parking bays (i.e. a maximum of 2m wide without wing mirrors.

Account must be taken of the potential for third party servicing occurring on the opposite side of the street or on adjacent frontages.

In circumstances where 3m clear roadway width could be achieved by parking suspensions or the use of narrow body vehicles, we will require the latter.

On Mews streets routine manoeuvring by HGVs adjacent to the threshold of neighbouring houses will not be acceptable.

Please refer to:
Appendix C: Site Layout Plan
Appendix D: Spoil Removal Plan
Appendix E: Plant & Material Delivery Plan
Appendix F: Concrete Delivery Plan

CONSTRUCTION TRAFFIC MANAGEMENT PLAN (CTMP)

Q12 Please confirm that a minimum of 3m clear roadway width will be maintained at all times to prevent the road becoming blocked.

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If it is not possible to maintain 3m clear roadway width and there is no alternative to the street becoming blocked during scheduled deliveries, please detail the management measures to be followed to ensure:

- Pedestrian passage is maintained at all times.
- Vehicular access to adjacent properties is maintained at all times.
- Emergency Access is maintained at all times.
- Domestic and commercial waste collections are not disrupted.
- Motorists are adequately forewarned of the blockage with appropriate signage on the approaches in conformance with Chapter 8 of the current TSRGD (Traffic Signs Regulations and General Directions).
- Trees do not become damaged.
- Vehicles will not drive on the footway other than at dedicated access points.

We will only agree to road closures in exceptional circumstances. In most cases good traffic management should ensure that the frequency and duration of blockages is managed so that vehicular traffic can pass the site. We will only agree to blockages occurring between 9.30am and 3pm. The driver must stay with the vehicle at all times and be ready to move on request if vehicular access to a neighbouring property is required and no alternative is available or in the event of an emergency. Banksmen must be positioned on all approaches to the site to forewarn highway users and advise of alternative routes. You will need to establish the days and times of refuse collections and ensure that there is no conflict. These can be viewed [here](#)

- The minimum existing clear carriageway width within Bury Walk is 2.40m, it is confirmed that this existing clear carriageway width will be maintained at all times.
- It is confirmed that a minimum clear carriageway width of 3m will be maintained within Sydney Street.

Please refer to:

Appendix C: Site Layout Plan

Appendix D: Spoil Removal Plan

Appendix E: Plant & Material Delivery Plan

Appendix F: Concrete Delivery Plan

CONSTRUCTION TRAFFIC MANAGEMENT PLAN (CTMP)

Q13. Will vehicles enter and leave the site (Yes/ No)? If yes, please detail how vehicles will enter and leave the site?

If vehicular access is provided vehicles should be able to turn within the site and exit in a forward direction. Alternatively, vehicles may reverse in and drive out in forward gear. Trained banksmen MUST be provided at all times when vehicles are manoeuvring into and out of the site. As a minimum, banksmen should have a Site Access Traffic Marshal qualification. The swept path of the chosen manoeuvre must be shown on the site plan. Trained banksmen must ensure the safe passage of pedestrians and vehicular traffic in the street when vehicles are accessing or egressing from the site.

No, vehicles will enter the site. For the skip, a narrow-bodied vehicle will stop in front of the site on Bury Walk, and spoil removal will be carried out using a gantry with a wait-and-load system.

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Q14. Please provide full details of the method of spoil removal (wait & load, conveyor, grab, skip swap, etc.)?

We will only agree to a methodology that maintains 3m of clear roadway width. Where the maintenance of 3m clear roadway width is impossible, temporary blockages will only be permitted subject to stringent controls (see Q13). The use of the wait and load methodology means that the kerbside is available for parking at times when any parking suspensions do not apply. Grabbing spoil from the roadway or front gardens using a grab lorry blocks traffic and will not be acceptable in most locations. We will not agree to the placing of skips on streets that experience saturated parking conditions overnight (90% occupancy on residents' parking bays) and where alternative methods of spoil removal could reasonably be carried out. Details of recorded parking occupancy levels are available from the Council's Transport team. The chosen method of spoil removal must avoid damaging nearby trees, historic mews arches or street furniture.

Spoil will be removed using a conveyor and gantry system, with wait and load methodology. The gantry will be installed on the pavement, allowing spoil to be transferred directly from site to a wait-and-load vehicle. This approach avoids traffic disruption and eliminates the need for long-term kerbside occupation.

The gantry will be a raised structure positioned partly within the property boundary and partly on the pavement, supported by scaffold legs. A minimum clear headroom of 2.3m will be provided beneath a protected plywood soffit, allowing pedestrians to walk safely underneath without obstruction. Scaffold legs will be boxed/protected to maintain safe access, and only a narrow strip of pavement (approximately 5–10 cm) will be occupied, leaving the remaining width of the footway fully open for pedestrian use.

In addition, suitable measures will be implemented to prevent dust, debris, or spoil from falling onto the footway during construction activities, ensuring the safety and comfort of pedestrians at all times.

Spoil will be conveyed via the gantry and loaded directly into narrow width wait-and-load vehicle by involving some manual handling on the gantry. The wait and load vehicle will be a Suzuki Carry (or similar narrow-bodied vehicle less than 2m in width) which has dimensions L:3.4m, W: 1.47m, and will operate in line with existing parking bays as shown in the Spoil Removal Plan. This ensures that traffic flow will not be obstructed.

The chosen methodology avoids the use of grab lorries or skips, and spoil will be removed efficiently without causing disturbance to neighbours or damage to street furniture.

Please refer to:

Appendix D: Spoil Removal Plan

Q15. How will concrete be supplied to the site, where will the delivery lorries be located and for how long? Where will concrete pumps, whether static or mobile, be positioned? How will concrete be transferred across the footway? Please illustrate with a numbered and dated drawing annotated with dimensions.

We will only agree to the use of concrete wagons where a minimum of 3m of clear roadway width can be maintained. Otherwise concrete must be hand mixed on site. At all times safe pedestrian passage across the front of the site must be maintained. Appropriate ramping must be used if hoses are run across the footway in order to maintain pedestrian passage. Alternatively, a gantry should be used to secure the hose safely (at a minimum height of 2.3m) overhead.

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- *Concrete will be delivered to site via a combined concrete mixer and pump truck.*
- *The concrete mixer will be positioned within the designated loading/unloading area (x4 suspended parking bays) outside No. 32 Sydney Street which is also owned by the applicant.*
- *Concrete will be pumped into site, hoses will be run through No. 32 Sydney Street and into site which is directly to the rear of No. 32 Sydney Street.*
- *Where concrete hoses intersect the footway, they will be contained within appropriate ramps.*
- *All concrete delivery activities will be supervised by fully qualified and experienced Traffic Marshals.*

Please refer to:

Appendix B: Swept Path: Access & Egress Route 2

Appendix F: Concrete Delivery Plan

CONSTRUCTION TRAFFIC MANAGEMENT PLAN (CTMP)

Q16. Please confirm that the delivery and collection of scaffolding for the site will be undertaken in accordance with the requirements of this CTMP. Please confirm that scaffolding lorries will be positioned at the kerbside (or appropriately on mews) so that scaffolding deliveries and collections do not impact on highway operation. Please confirm that a sufficient number of parking suspensions will be sought to ensure that scaffold lorries are positioned so as not to interfere with traffic.

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SCHEDULING

Q17 Please confirm that no more than a single delivery vehicle associated with the development will be positioned on the highway in the vicinity of the site at any given time.
For basement extensions to residential properties we will not agree to there being more than a single vehicle on the highway servicing the site at any given time (save for when a concrete pump is being used in conjunction with a concrete wagon).

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For some large development schemes, and solely at the discretion of the Council, it might be appropriate for more than a single construction vehicle to be on the highway in the vicinity of the site at a given time. The maximum number of such vehicles simultaneously on the highway in the vicinity of the site must be specified and justified here.

Confirmed.

CONSTRUCTION TRAFFIC MANAGEMENT PLAN (CTMP)

Q18. In order to devise a robust scheduling strategy, the approximate number of construction vehicle movements necessary to complete the proposed development must be established. Please provide a robust estimate of the maximum number of vehicles visiting the site (daily or weekly) per vehicle type **during each major phase of the work**. For each vehicle type specify the vehicles' respective capacities and maximum dimensions (with and without wing mirrors). Please specify the maximum dwell time for each construction vehicle type.

The Council understands the exact number of construction vehicle movements cannot be known from the outset. However, the scheduling strategy must be sufficiently robust to satisfactorily deal with the construction traffic volumes that do arise. Accordingly, maximum vehicle sizes and maximum dwell times for each construction vehicle type must be set to ensure conflicting deliveries never arise and to maintain highway operation.

Table 2: Anticipated start date and expected duration of works

START DATE	DURATION
1 st September 2025	30 weeks

Table 3: Site activities and estimated vehicle movements

SITE ACTIVITY	DURATION WEEKS	VEHICLE MOVEMENTS PER WEEK			
		WAIT & LOAD VEHICLE	COMBINED CONCRETE MIXER AND PUMP TRUCK	NARROW BODIED VEHICLE	TOTAL (AVG) PER WEEK
1. Site set up and demolition	1	1	0	5	6
2. Basement excavation and piling	6	6	0.5	2	8.5
3. Sub-structure	6	6	0.5	3	9.5
4. Super-structure	7	0	0	4	4
5. Cladding	3	0	0	4	4
6. Fit-out, testing, commissioning	7	0	0	3	3

Table 4: Construction vehicle descriptions

VEHICLE DESCRIPTION	LENGTH (M)	WIDTH (M)	DWELL TIME (MINS)
Wait & Load Vehicle	3.4	1.47	30
Combined Concrete Mixer and Pump Truck	10.50	2.80	45
Narrow Bodied Vehicle	4.90	1.90	45

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Table 5: Expected abnormal loads for the duration of the project

PROGRAMME STAGE	DURATION (MINUTES)	MAX NO OF VEHICLES/DAY	VEHICLE TYPE
1. Site set up and demolition	N/A	N/A	N/A
2. Basement excavation and piling	N/A	N/A	N/A
3. Sub-structure	N/A	N/A	N/A
4. Super-structure	N/A	N/A	N/A
5. Cladding	N/A	N/A	N/A
6. Fit-out, testing, commissioning	N/A	N/A	N/A

CONSTRUCTION TRAFFIC MANAGEMENT PLAN (CTMP)

VEHICLE CALL UP PROCEDURE

Q19. Please confirm you accept the below requirements.

- All deliveries shall be pre booked and allocated set arrival times.
- Delivery instructions shall be sent to all suppliers and contractors including the maximum dwell times specified above.
- Suppliers shall call the site a minimum of 20mins before their vehicle arrives at site to confirm that the loading area is available.
- If the loading area is unavailable construction vehicles shall not proceed to the site.
- Vehicles shall not wait or stack on any road within the Royal Borough.
- The loading/collection area shall be clear of vehicles and materials before the next lorry arrives.
- Contractors' vehicles shall not park in any suspended parking bays or on suspended waiting and loading restrictions.
- The engines of contractors' vehicles shall not be kept idling.

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CONSTRUCTION TRAFFIC MANAGEMENT PLAN (CTMP)

Q20. How will you protect pedestrians during the construction works, particularly vulnerable users?

Vulnerable footway users include wheelchair users, the elderly, people with walking difficulties, young children, people with prams, blind and partially sighted people, etc. A secure hoarding will be required to the site boundary with a lockable access. Any work above ground floor level may require a covered walkway adjacent to the site. A licence must be obtained for scaffolding and gantries. The adjoining public highway must be kept clean and free from obstructions. Lighting and signage must be used on temporary structures/ skips/ hoardings, etc. Appropriate ramping must be used if cables, hoses, etc. are run across the footway. A banksman must be in position on the footway during the transfer of materials across the footway to ensure that safe pedestrian passage is maintained. As a minimum, banksmen should have a Site Access Traffic Marshal qualification.

In order to protect pedestrians and vulnerable users, the following measures will be undertaken:

- No construction work will be completed in public areas and the adjoining footway and highway will be kept clear and free of obstructions.
- A secure hoarding will be erected around the boundary of the site, as required, and will feature lockable inwardly opening plant/material and pedestrian site access gates.

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- All vehicle movements will be supervised by suitably qualified and experienced Traffic Marshals who will manage the interaction of construction vehicles and pedestrians, vulnerable footway users, cyclists and other road users. Whenever necessary, activities will be stopped to allow pedestrians to pass safely.
- Vulnerable pedestrians will take priority and, if required, will be escorted on the footpath across the site access. Traffic Marshalls will possess the Site Access Traffic Marshal qualification as a minimum.
- During all stages of the development vulnerable users including wheelchair users and those with prams will be afforded every assistance possible to safely navigate the area and this may mean stopping traffic briefly to provide as much space as is required by the pedestrian.
- Traffic Marshals will ensure that pedestrians and vulnerable users are clear of the site prior to releasing or accepting a vehicle from/to site.
- Traffic Marshals will be in position on the footway to ensure that safe pedestrian passage is maintained during the transfer of spoil, plant and materials across the footway.

Q21 Please confirm that the operators of the construction vehicles servicing the site have achieved FORS Silver accreditation to demonstrate your commitment to using clean safe vehicles with good levels of direct vision, safety bars and advisory signage.

The Council expect operators of construction vehicles to have achieved Freight Operators Recognition Scheme Silver accreditation to satisfy Council Policies on road safety (CT1 (h)) and air quality (CE5) .Details can be found [here](#).

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CONSTRUCTION TRAFFIC MANAGEMENT PLAN (CTMP)

HIGHWAY LICENCES

Q22. Please confirm if you intend to erect hoarding or scaffolding on the highway or to use the highway for construction activity, for the storage of plant or materials or for welfare facilities. If so you need a highways licence and to lodge an agreed bond with the Council. Details on how to apply for highways licences are available [here](#).

Use of highway for storage or welfare facilities is at the discretion of the Council and is generally not permitted. If you propose to place items on the highway you must supply full justification, setting out why it is impossible to allocate space on-site. The provision of hoarding around trees, street furniture or historic arches requires a highways licence.

No hoarding is required.

Scaffolding to be used during the groundworks phase, the main contractor to obtain required license.

Q23 . Please confirm if you intend to place a skip on the highway. If so you need a skip licence, even if the skip is only placed on the highway surface for a short period. A skip licence is distinct from a highways licence to place materials or temporary structures. Two licences may be required. Details on how to apply for a skip licence are available [here](#).

No, a skip will not be placed on the highway.

Q24 Please confirm if you intend to close a footway at any stage during the construction works?
Permission will not be given to close footways unless this is unavoidable. Where a footway closure is proposed please submit a scaled plan of the proposed diversion route showing key dimensions. Please provide details of all safety signage, barriers and accessibility measures such as ramps and lighting etc. All such signage must conform to Chapter 8 of the current TSRGD and be compliant with NRSWA regulations.

No footways will be closed.

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Q25 Please confirm if you intend to close a roadway at any stage during the construction works? Details on how to apply for a road closure are available [here](#).

The Council will only agree to traffic diversion that we consider necessary and for the minimum duration. If a traffic diversion is proposed, you should submit detailed dated and numbered plans showing the impact on the surrounding highway network including the extent of the closure; the proposed diversion route for vehicular traffic and pedestrians; traffic management; the affected waiting/loading restrictions; affected parking facilities; emergency services access; public transport; refuse collection; deliveries; local businesses; etc. Temporary Traffic Management Orders and consultation will require an 8-week lead-in time.

No roadways will be closed.

Q26 Please confirm if you intend to carry out any crane lifts during the construction works. If so a mobile lifting operation licence will be required. Details of how to apply for a mobile lifting operation licence are available [here](#).

No crane lifts are required.

Q27 Please confirm whether a temporary crossover is required to enable construction vehicles to access the site or if you will need to protect the highway surface from outriggers, heavy or point loads or other equipment? If so you require a temporary crossover licence and to lodge an agreed bond with the Council. Details of how to apply for a temporary crossover licence are available [here](#).

The Borough's footways are not engineered to take heavy loading from construction vehicles. A temporary crossover licence must be obtained where either a new crossover is required for a temporary period for construction access or where construction vehicles are to cross the footway using an existing crossover. Under such a licence a suitable crossover can be provided for a temporary period after which the footway will be reinstated by the Council at the expense of the licence holder. The use of metal plates on the highway is not acceptable. The use of vehicles with outriggers on the highway requires a licence.

No, a temporary crossover will not be needed.

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PARKING SUSPENSIONS

Q28 Will you require a parking suspension to facilitate the development? How many parking suspensions do you require? Please annotate the number of bays and/ or length of suspension required on the site access plan. Please specify the frequency and duration of the suspensions and identify what they are for e.g. loading, access, storage. Please provide justification for all intended parking bay suspensions.

Details on how to apply for parking suspensions are available [here](#).

The number of parking bay suspensions and the duration and frequency of those suspensions must be the minimum necessary to carry out the development while maintaining at least 3m of clear roadway for vehicular passage. Parking bay suspensions are normally only permitted outside the property being redeveloped. The Council will only agree to suspend disabled bays, doctors' bays, car club bays or diplomats' bays if there is no alternative. Parking bay suspensions will only apply during construction traffic hours except where an associated skip or hoarding licence has been issued. Skips should be removed to leave the highway clear over the weekend. Once the CTMP is agreed you will need to apply to the Council's Parking Section for the parking suspensions specified within this plan. Suspended parking bays are not to be used for simple parking convenience and the developer must ensure their contractors travel to the site using public transport, on foot or by bicycle. Contractors' vehicles will not be permitted to park in suspended parking bays. Bays are suspended for operational purposes only. We expect developers to let the Council know if they complete any task(s) earlier than anticipated and no longer need the suspended bays that they have reserved; so we can return them to use at the earliest opportunity.

- a.** One parking bay suspension (5m length) is required within Bury Walk , this parking bay suspension is only required during spoil removal activities.
- b.** Four parking bay suspensions (20m length) are required within Sydney Street to facilitate concrete delivery, these parking bay suspensions are only required during concrete delivery activities.

Please refer to:

- Appendix C: Site Layout Plan
- Appendix D: Spoil Removal Plan
- Appendix F: Concrete Delivery Plan

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GENERAL MANAGEMENT ISSUES

Q29. Please confirm you accept the below requirements:

- The depositing of mud/detritus on the highway originating from the site or from any construction vehicle associated with the development is unacceptable.
- Under no circumstances should concrete residue or other detritus be washed into the drainage system. Appropriate measures will be taken to prevent concrete and other detritus from being washed into the public highway drainage system. The Council will be informed promptly should any such damage to the highway occur and will be duly reimbursed for the cost of the repairs.
- A wheel washing facility shall be provided at all vehicular access gates to the development site to ensure that mud/detritus originating from the site is not deposited on the public highway.
- Where the deposition of some dirt on the highway is unavoidable, any mud/detritus shall be expeditiously cleared using street cleansing vehicles or similar. Jet washers will not be used to flush such dirt into road gullies. No development dirt shall be evident on the highway at the end of any working day

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- Q30. Please confirm that you will make all reasonable efforts and always when specifically directed by the Council to coordinate the scheduling of construction traffic movement with other nearby developments and those on the construction traffic routes specified above. Please identify relevant development sites with which you will coordinate.

When more than one development is occurring on a narrow street or on cul de sacs where access is constrained, deliveries to development sites must be coordinated so as to maintain access at all times and minimise disruption.

It is confirmed that instructions will be followed from the Council with regards to coordination and scheduling of construction traffic. Contact with the Councils Highways Department will be continued throughout the duration of the construction phase of the scheme to ensure compliance with the approved CTMP and to alleviate any potential disturbance from construction traffic.

To minimise the cumulative impact on the local road network the Principal Contractor will liaise with contractors completing work on any adjacent sites to enable the scheduling of delivery and waste removal vehicles and to avoid the presence of more than one construction vehicle on the highway at any one time. Following a search of the RBKC Planning Portal for relevant projects granted planning permission within Pond Place, Bury Walk and Sydney Street the sites detailed below have been identified as potential sites of interest.

It is acknowledged that the list below is not exhaustive, therefore the Principal Contractor will undertake a review of the surrounding area two weeks prior to commencement of works to identify any additional active sites.

Table 6: Potential sites of interest

ADDRESS	PLANNING REF	PROPOSED DEVELOPMENT
Pond House, Pond Place,	PP/23/06196	Installation of vehicular and pedestrian gate including access control system to front of Pond House.
16B Bury Walk	PP/24/01057	Construction of single storey basement extension; erection of additional first floor with rear mansard; creation of terrace at first floor level; 2 x rooflights; replacement front windows with timber sash windows; rear entrance door to be moved to side passage; internal reconfiguration throughout.
20 Sydney Street/Bury Walk	PP/23/04503	Erection of two storey "granny annex" and basement extension fronting Bury Walk and alteration to rear stair and garden of 20 Sydney Street. (Amendment to planning permission ref: PP/21/03270 to provide basement).
41 Bury Walk	PP/23/07597	Demolition of existing house and construction of a house at ground and first floor levels.
17 Bury Walk	PP/23/01518	Single storey rear extension, minor alterations to facade of rear outrigger and fenestration alterations to rear exterior wall.
First/second Floor Flat, 49 Sydney Street	PP/24/06317	Erection of mansard roof extension with alterations to front & rear elevations and internal 3rd floor; as well as reinstatement of historic layout and features to 1st and 2nd floors.

CONSTRUCTION TRAFFIC MANAGEMENT PLAN (CTMP)

Flat C, 33 Sydney Street	PP/23/02530	Creation of roof terrace to rear above existing closet wing extension at second floor level.
13 Sydney Street	PP/22/01842	Erection of mansard roof addition; extension of the closet wing at second floor level; removal of garage within rear garden, reinstatement of garden; replacement of garage door with new one; removal of garden steps; alterations to, and provision of new fenestration to rear elevation and associated internal alterations.
Upper Flat, 47 Sydney Street	PP/21/06978	Refurbishment of maisonette internally and externally including replacement of timber decking and re-painting of railings on rear terrace; replacement of tiles on entrance steps; replacement of entrance door and replacement of entrance overhead light with 2no. wall lights.

Q31 Please confirm that if directed by the Council you will monitor any site accesses or loading areas authorised by this plan with CCTV between 8am and 6pm Monday to Friday and to make any footage available on request.

<i>Please delete as appropriate</i>	Y
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Q32 Please identify who is responsible for the day to day implementation of this CTMP and provide their contact details. This person must be responsible for the supervising, controlling and monitoring vehicle movements to/from the site and coordinating and allocating time slots.
Notwithstanding the details given hereunder the developer/ owner will necessarily, as a condition of their planning permission, be responsible for ensuring this plan is adhered to in full.

<p>Name: Mr. Alvydas Norvaisas (Alex)</p> <p>Position: Site Manager</p> <p>Address: 210/210A Billet Road., London, E17 5DY</p> <p>Tel: 07950 563 852 / 0207 3818 089</p> <p>Email: enquiries@thamesviewconstruction.co.uk</p>

CONSTRUCTION TRAFFIC MANAGEMENT PLAN (CTMP)

PROGRAMME/KEY DATES (FOR INFORMATION)

Q33 Please supply a broad-brush programme and total timescale for the project, giving the duration of each major phase of the construction and the anticipated start date if known.

The Council understands the exact duration of the development works cannot be known from the outset. Nevertheless, an approximate programme is required to properly inform residents and to assist in the management of cumulative development impacts.

Table 7: Broad-brush programme of works subjects to planning

SITE ACTIVITY	DURATION	START DATE	END DATE
1. Site set up and demolition	1	1st September 2025	7 th September 2025
2. Basement excavation and piling	6	8 th September 2025	19 th October 2025
3. Sub-structure	6	20 th October 2025	30 th November 2025
4. Super-structure	7	1 st December 2025	2 nd February 2026
5. Cladding	3	3 rd February 2026	23 rd February 2026
6. Fit-out, testing, commissioning	7	24 th February 2026	13 rd April 2026*

*INCLUDES CHRISTMAS SHUTDOWN ON 21ST DECEMBER 2025 THEN RESTART ON 5TH JANUARY 2026.

CONSTRUCTION TRAFFIC MANAGEMENT PLAN (CTMP)

SUMMARY CTMP

Q34 . Please confirm that you have completed the summary sheet on the following page and please confirm that should this plan be approved by the Council you will affix this summary sheet in a position prominent at the front boundary of the site for the full duration of your development works.

<i>Please delete as appropriate</i>	Y
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CONSTRUCTION TRAFFIC MANAGEMENT PLAN (CTMP)

SUMMARY OF CTMP REQUIREMENTS

SITE ADDRESS

32 BURY WALK, LONDON, SW3 6QB

PLANNING REF. NO. (to be filled in once a full CTMP is approved)

A SAFE PEDESTRIAN ROUTE MUST BE KEPT OPEN ADJACENT TO THE SITE

THE ROADWAY IN FRONT OF THE SITE MUST BE KEPT PASSABLE

PERMITTED CONSTRUCTION TRAFFIC HOURS (Monday to Friday only)

PERMITTED HOURS FOR DELIVERIES AND COLLETCTIONS	
Monday – Friday (During Term Time)	09:30 – 15:00
Monday – Friday (Outside Term Time)	09:30 - 16:30
Saturdays	Not Permitted
Sundays	Not Permitted

MAXIMUM NUMBER/LENGTH (in distance)
OF PARKING SUSPENSIONS REQUIRED

Maximum Number: 5
Length: 25m

HIGHWAY LICENCES REQUIRED (Please tick as appropriate)

Temporary Structure (including hoarding and scaffolding)

N/A

Skip Licence

N/A

Temporary Crossover Licence

✓

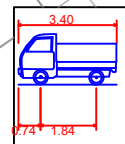
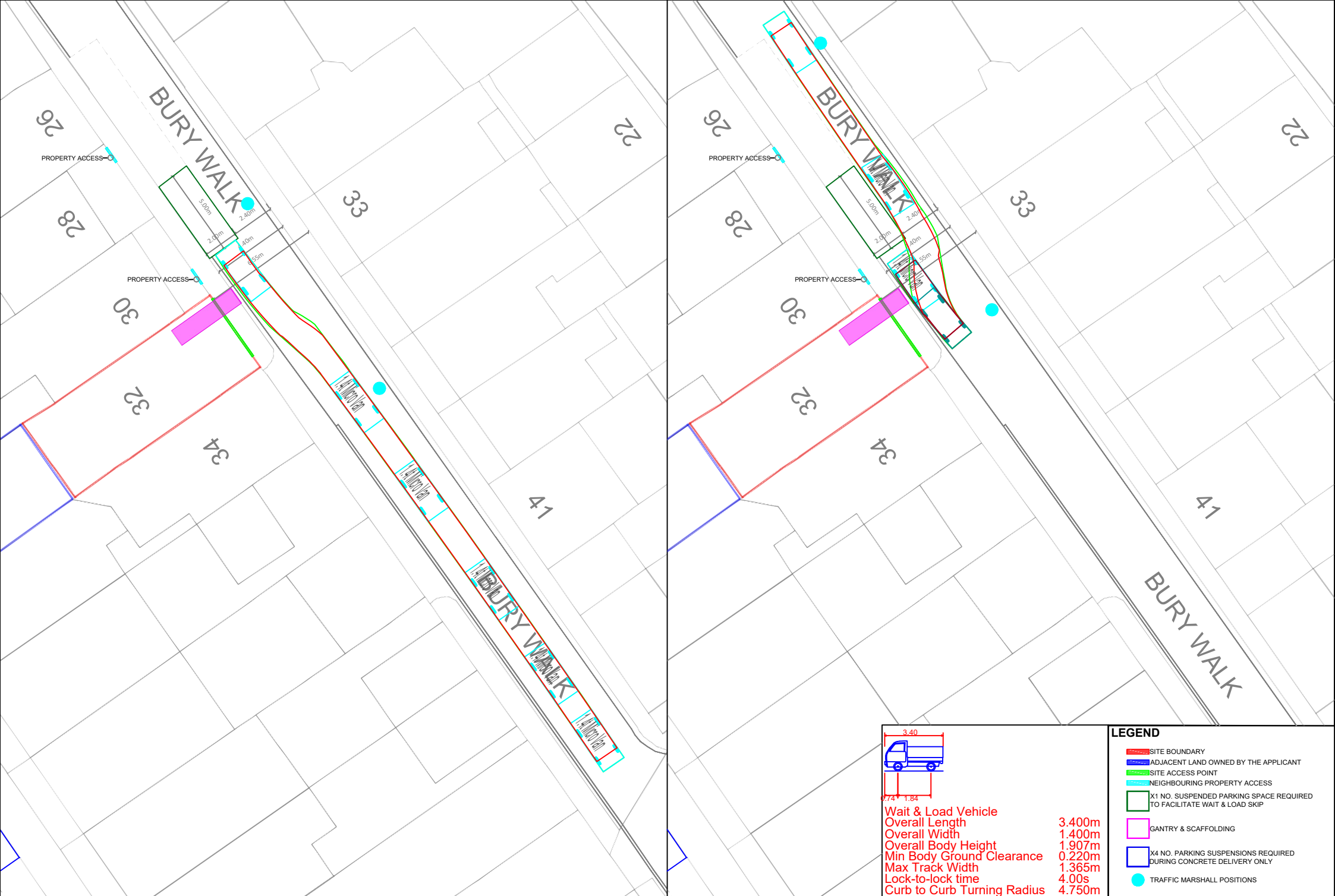
Mobile Lifting Operation Licence

N/A

CTMP APPENDICES

APPENDIX A

SWEPT PATH: ACCESS & EGRESS ROUTE 1

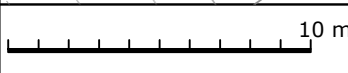


Wait & Load Vehicle	
Overall Length	3.400m
Overall Width	1.400m
Overall Body Height	1.907m
Min Body Ground Clearance	0.220m
Max Track Width	1.365m
Lock-to-lock time	4.00s
Curb to Curb Turning Radius	4.750m

LEGEND	
	SITE BOUNDARY
	ADJACENT LAND OWNED BY THE APPLICANT
	SITE ACCESS POINT
	NEIGHBOURING PROPERTY ACCESS
	X1 NO. SUSPENDED PARKING SPACE REQUIRED TO FACILITATE WAIT & LOAD SKIP
	GANTRY & SCAFFOLDING
	X4 NO. PARKING SUSPENSIONS REQUIRED DURING CONCRETE DELIVERY ONLY
	TRAFFIC MARSHAL POSITIONS

SPA : Site Access & Egress 1

Scale 1:250 on A4

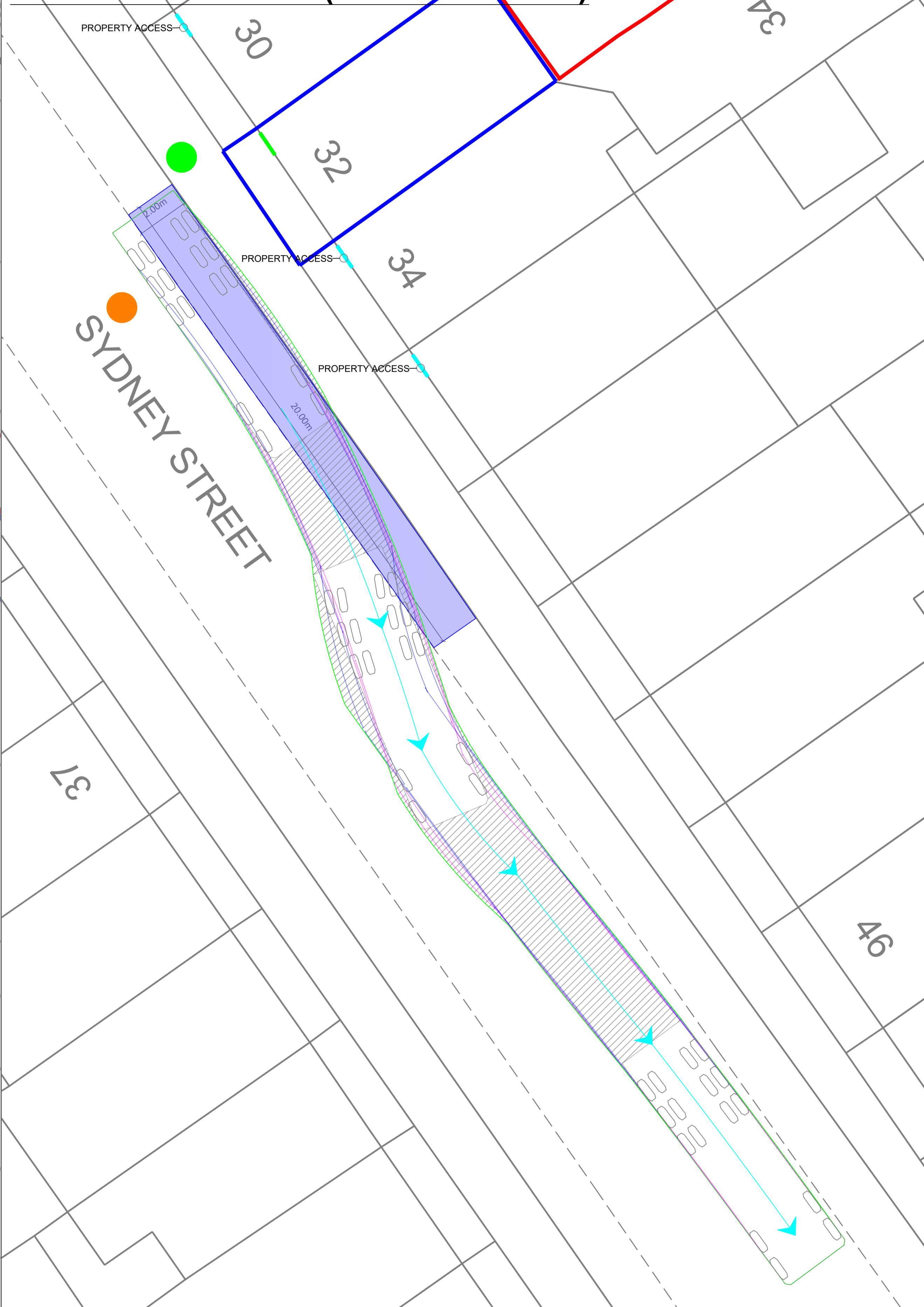


APPENDIX B

SWEPT PATH: ACCESS & EGRESS ROUTE 2

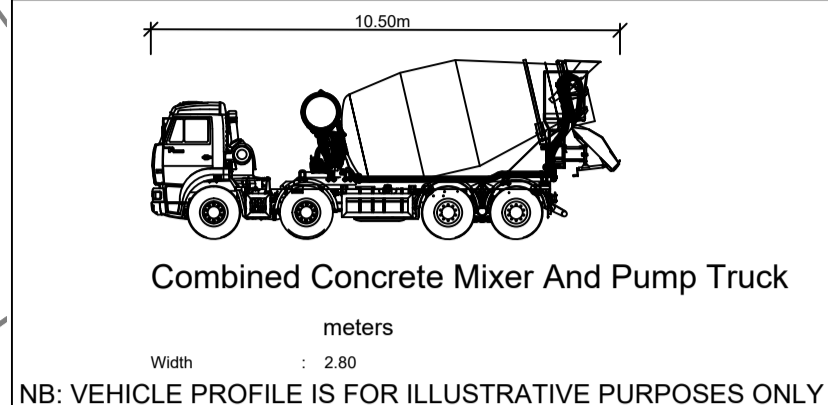
SITE ACCESS: ROUTE 2 - COMBINED CONCRETE MIXER AND PUMP TRUCK (10.50M X 2.80M)

SITE EGRESS: ROUTE 2 - COMBINED CONCRETE MIXER AND PUMP TRUCK (10.50M X 2.80M)



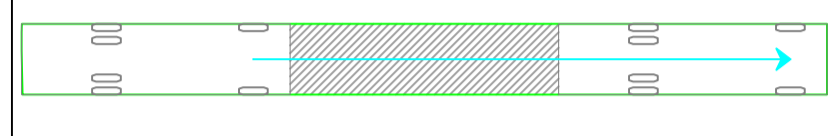
REV:	DETAILS:
V1	SWEPT PATH: ACCESS AND EGRESS ROUTE 2 (25.02.25)

VEHICLE DETAILS:

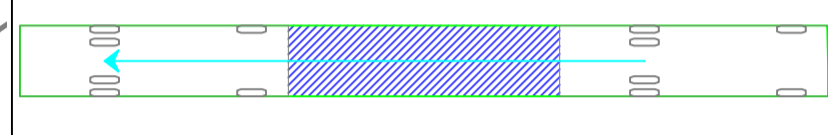


NB: VEHICLE PROFILE IS FOR ILLUSTRATIVE PURPOSES ONLY

FORWARD MOVEMENTS ARE SHOWN IN GREY
(design speed for all constrained forward movements - 3mph)



REVERSE MOVEMENTS ARE SHOWN IN BLUE
(design speed for all reverse movements - 2mph)



KEY:

- SITE BOUNDARY
- ADJACENT LAND OWNED BY THE APPLICANT
- SITE ACCESS POINT
- NEIGHBOURING PROPERTY ACCESS
- X1 NO. SUSPENDED PARKING SPACE REQUIRED TO FACILITATE SKIP ACCESS AND EGRESS
- SKIP POSITION
- X4 NO. PARKING SUSPENSIONS REQUIRED DURING CONCRETE DELIVERY ONLY
- TRAFFIC MARSHAL 1 POSITION
- TRAFFIC MARSHAL 2 POSITION

NOTES:

- a. Do not scale from this drawing.
- b. This drawing is to be read and printed in colour.
- c. This drawing is for illustrative purposes only.
- d. Road layout and location of street furniture is approximate.
- e. Swept Path Analysis drawings are indicative only, it remains the Principal Contractor/Freight Operators responsibility to ensure that vehicles are able to undertake all proposed site access and egress manoeuvres and the required manoeuvres for the entirety of the proposed local site access and egress route.
- f. This indicative design is for guidance purposes only. Seneca Services accepts no liability for any damage, loss, expenses, or cost incurred as a result of relying on the information provided in the design. The design has been derived from a combination of third-party information and the application of Seneca Services reasonable skill and care but may be subject to other such information and variations of which Seneca Services is unaware. Therefore, use of the information is entirely at your own risk.



Seneca Services Ltd
Contact: Mark Edgar
E: mark@senecaservices.co.uk
T: 07545 898 726
W: www.senecaservices.co.uk

CLIENT:
AV REAL ESTATE

PROJECT:
32 BURY WALK, LONDON SW3 6QB

DRAWING TITLE:
SWEPT PATH: ACCESS AND EGRESS ROUTE 2

DRAWING STATUS:
FOR INFORMATION

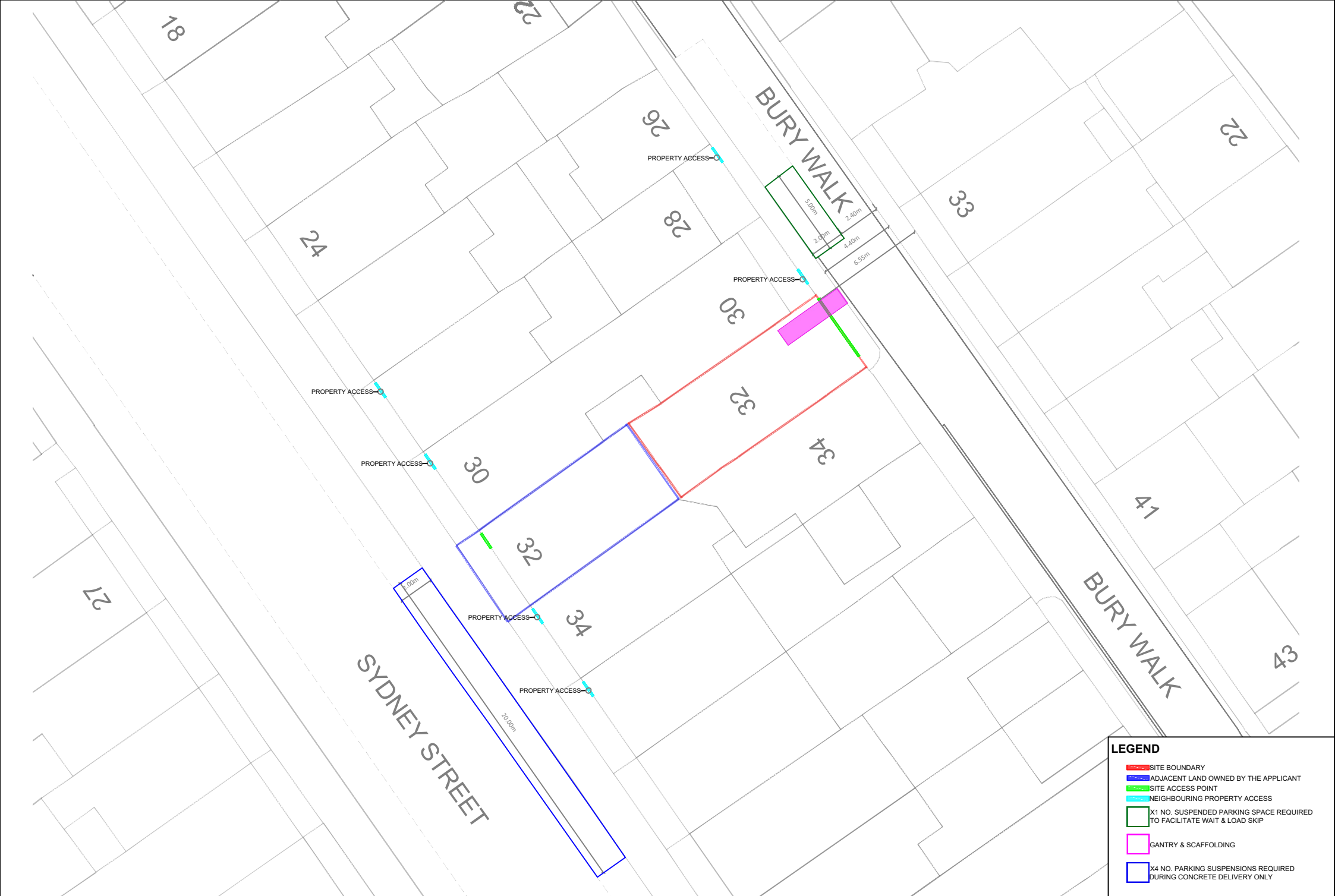
DRAWN:	DESIGNED:	DATE:	SCALE:	SIZE:
ME	ME	25.02.25	1:100	A1

DRAWING NUMBER:	REV:
DN-513	V1

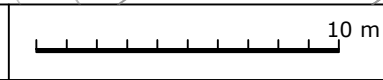
NB: Swept Path Analysis drawings are indicative only, it remains the Principal Contractor/Freight Operators responsibility to ensure that vehicles are able to undertake all proposed site access and egress manoeuvres and the required manoeuvres for the entirety of the proposed local site access and egress route.

NB: Swept Path Analysis drawings are indicative only, it remains the Principal Contractor/Freight Operators responsibility to ensure that vehicles are able to undertake all proposed site access and egress manoeuvres and the required manoeuvres for the entirety of the proposed local site access and egress route.

APPENDIX C
SITE LAYOUT PLAN

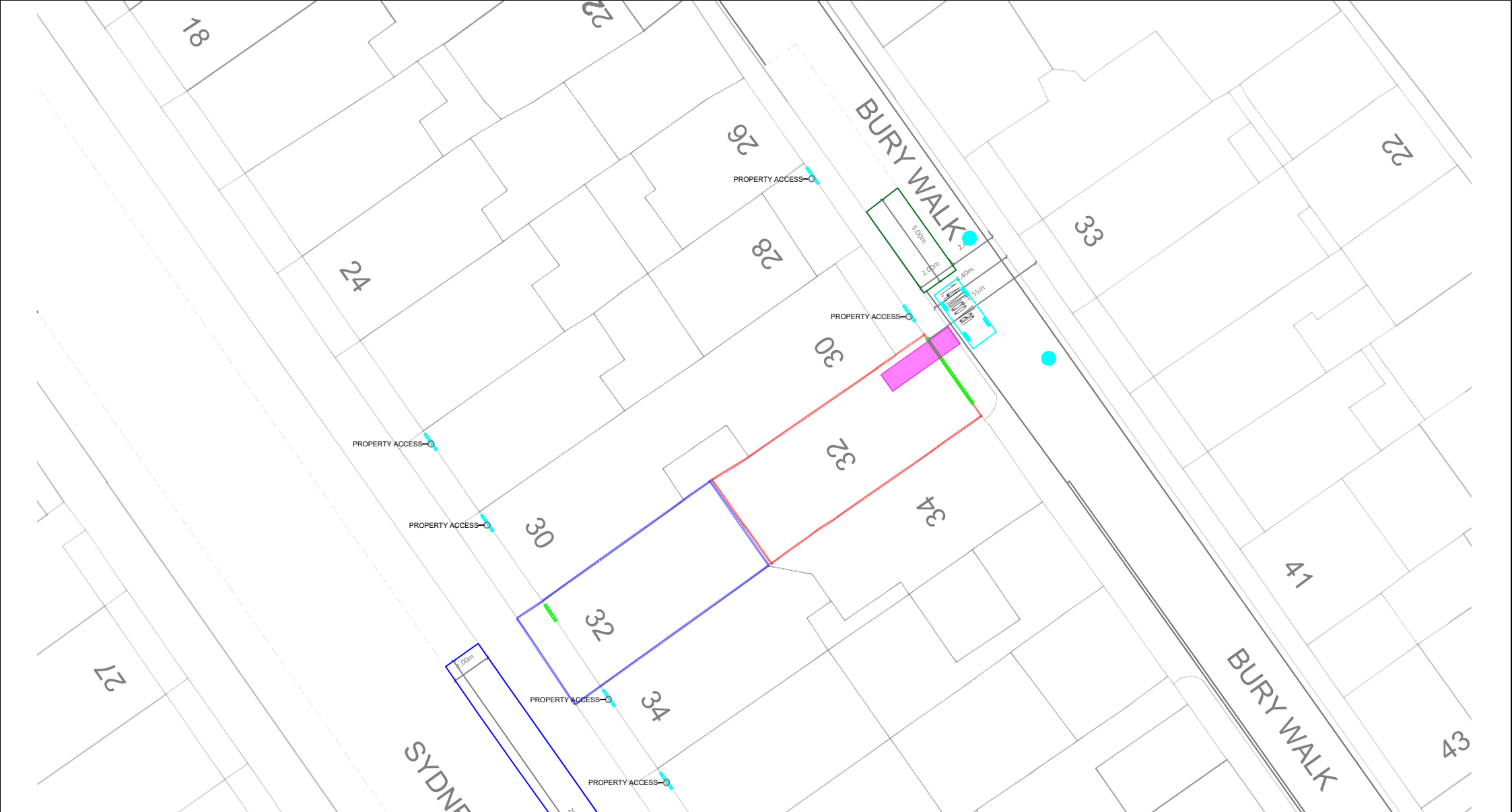


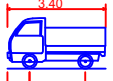
LEGEND	
	SITE BOUNDARY
	ADJACENT LAND OWNED BY THE APPLICANT
	SITE ACCESS POINT
	NEIGHBOURING PROPERTY ACCESS
	X1 NO. SUSPENDED PARKING SPACE REQUIRED TO FACILITATE WAIT & LOAD SKIP
	GANTRY & SCAFFOLDING
	X4 NO. PARKING SUSPENSIONS REQUIRED DURING CONCRETE DELIVERY ONLY











APPENDIX D

SPOIL REMOVAL PLAN



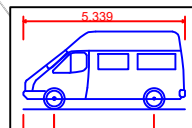
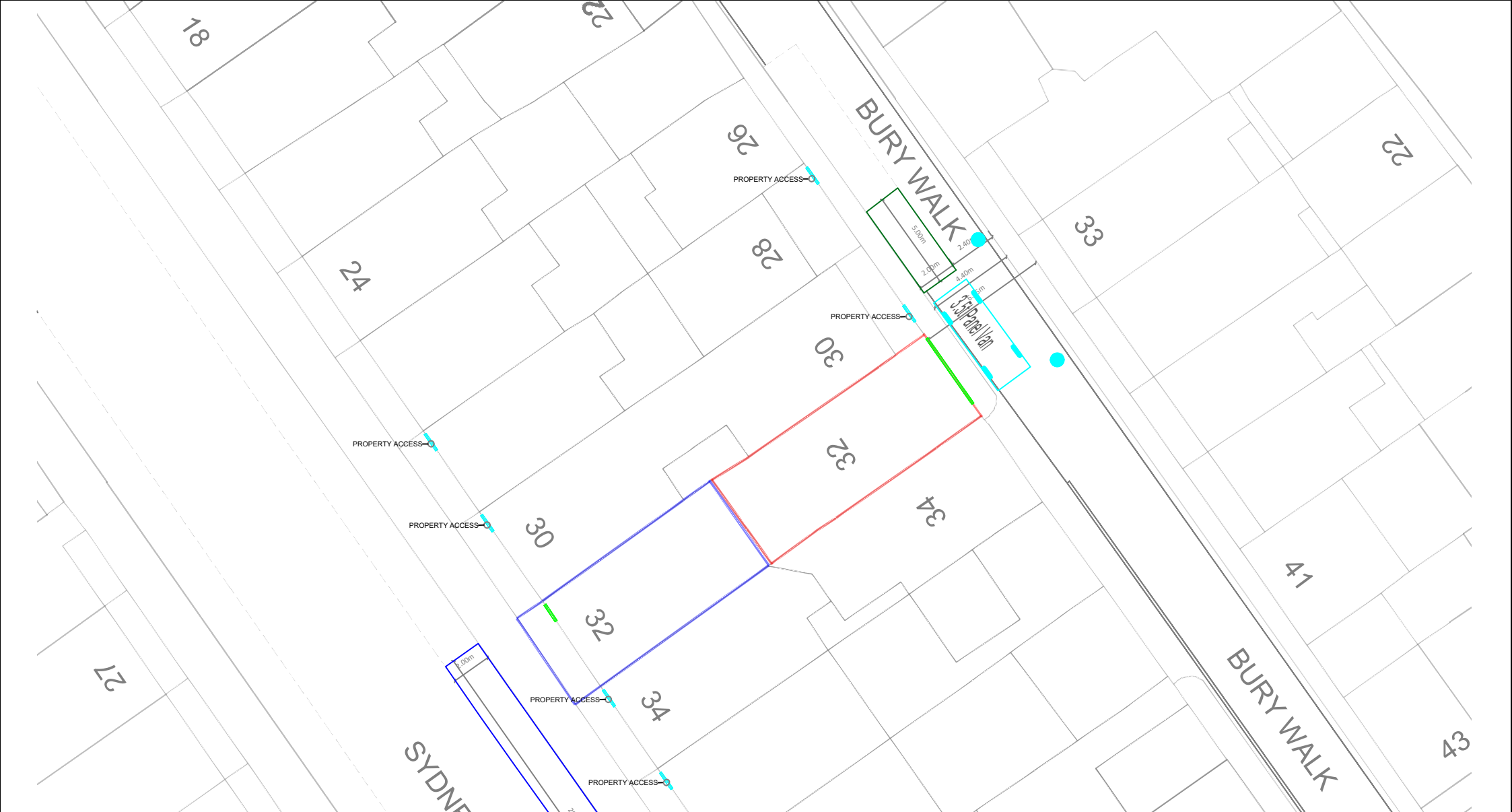
	
3.40	
1.84	
0.74	
Wait & Load Vehicle	
Overall Length	3.400m
Overall Width	1.400m
Overall Body Height	1.907m
Min Body Ground Clearance	0.220m
Max Track Width	1.365m
Lock-to-lock time	4.00s
Curb to Curb Turning Radius	4.750m

LEGEND	
	SITE BOUNDARY
	ADJACENT LAND OWNED BY THE APPLICANT
	SITE ACCESS POINT
	NEIGHBOURING PROPERTY ACCESS
	X1 NO. SUSPENDED PARKING SPACE REQUIRED TO FACILITATE WAIT & LOAD SKIP
	GANTRY & SCAFFOLDING
	X4 NO. PARKING SUSPENSIONS REQUIRED DURING CONCRETE DELIVERY ONLY
	TRAFFIC MARSHALL POSITIONS



APPENDIX E

PLANT & MATERIAL DELIVERY PLAN



3.5t Panel Van
 Overall Length 5.339m
 Overall Width 1.986m
 Overall Body Height 2.565m
 Min Body Ground Clearance 0.338m
 Track Width 1.986m
 Lock-to-lock time 4.00s
 Curb to Curb Turning Radius 6.400m

LEGEND	
	SITE BOUNDARY
	ADJACENT LAND OWNED BY THE APPLICANT
	SITE ACCESS POINT
	NEIGHBOURING PROPERTY ACCESS
	X1 NO. SUSPENDED PARKING SPACE REQUIRED TO FACILITATE WAIT & LOAD SKIP
	X4 NO. PARKING SUSPENSIONS REQUIRED DURING CONCRETE DELIVERY ONLY
	TRAFFIC MARSHALL POSITIONS